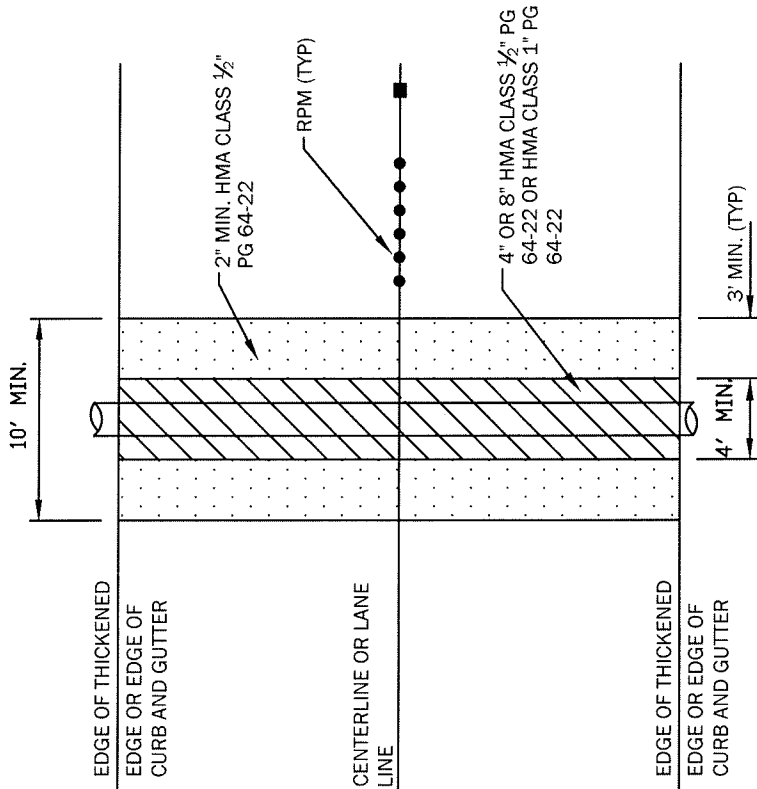


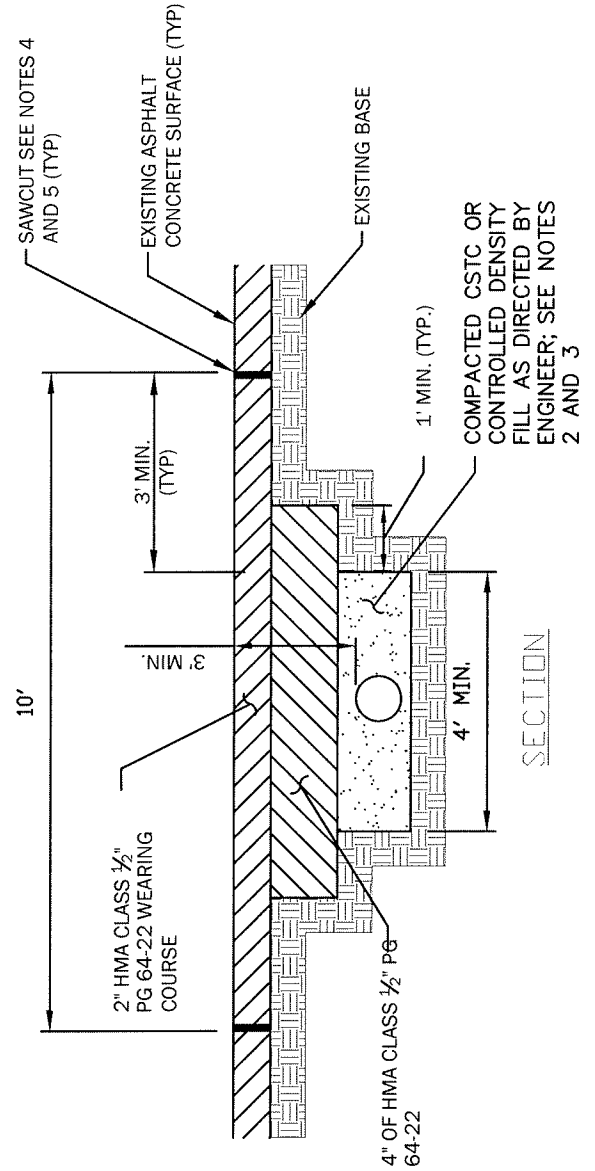
NOTES:

1. ASPHALT CONCRETE MIX SHALL BE HMA CLASS 1/2" OR CLASS 1" PG 64-22.
2. ALL TRENCH BACKFILL SHALL BE CSTC OR CONTROLLED DENSITY FILL.
3. CONTROLLED DENSITY FILL SHALL MEET WSDOT STANDARDS AS STATED IN 2-09.3(1)E OF THE STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION MANUAL M41-10, CURRENT EDITION.
4. ALL SAW CUTS SHALL BE VERTICAL AND IN STRAIGHT LINES AS DIRECTED BY ENGINEER.
5. TACK ASPHALT FACES OF SAW CUTS AND SEAL SAW CUTS WITH PG 64-22 OIL.
6. HOT MIX ASPHALT SHALL BE A MINIMUM OF 6 INCHES THICK ON LOCAL STREETS.
7. PAVING FABRIC (IF FOUND) WILL NOT REQUIRE REPLACEMENT.



2" DEPTH OF GRIND OR SAWCUT

PLAN



SECTION

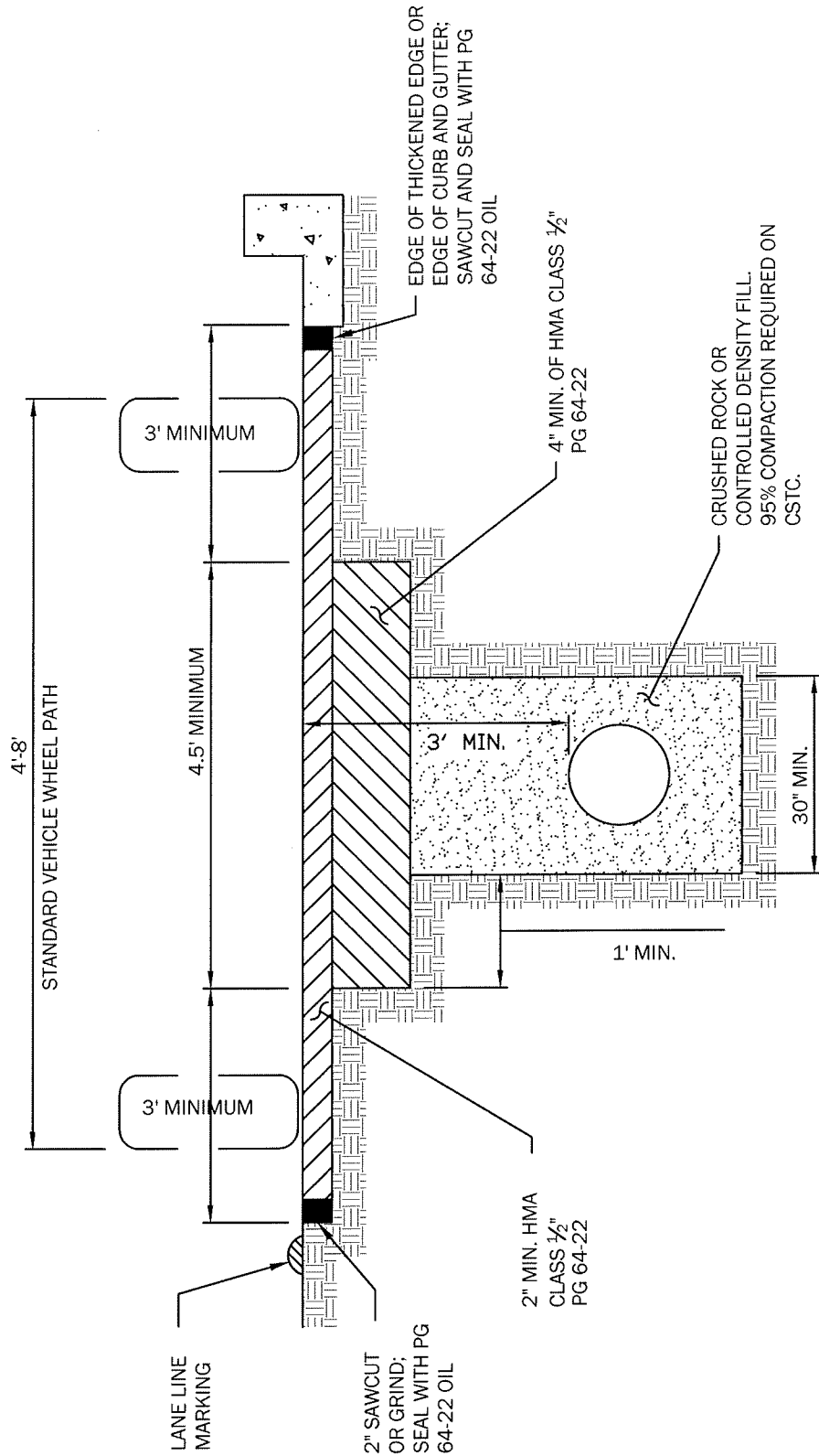
TOWN OF YARROW POINT

4030 95TH AVENUE NE
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ASPHALT PAVEMENT PATCHING AND RESTORATION DETAILS

RD-13a



LESS THAN FULL WIDTH OVERLAY

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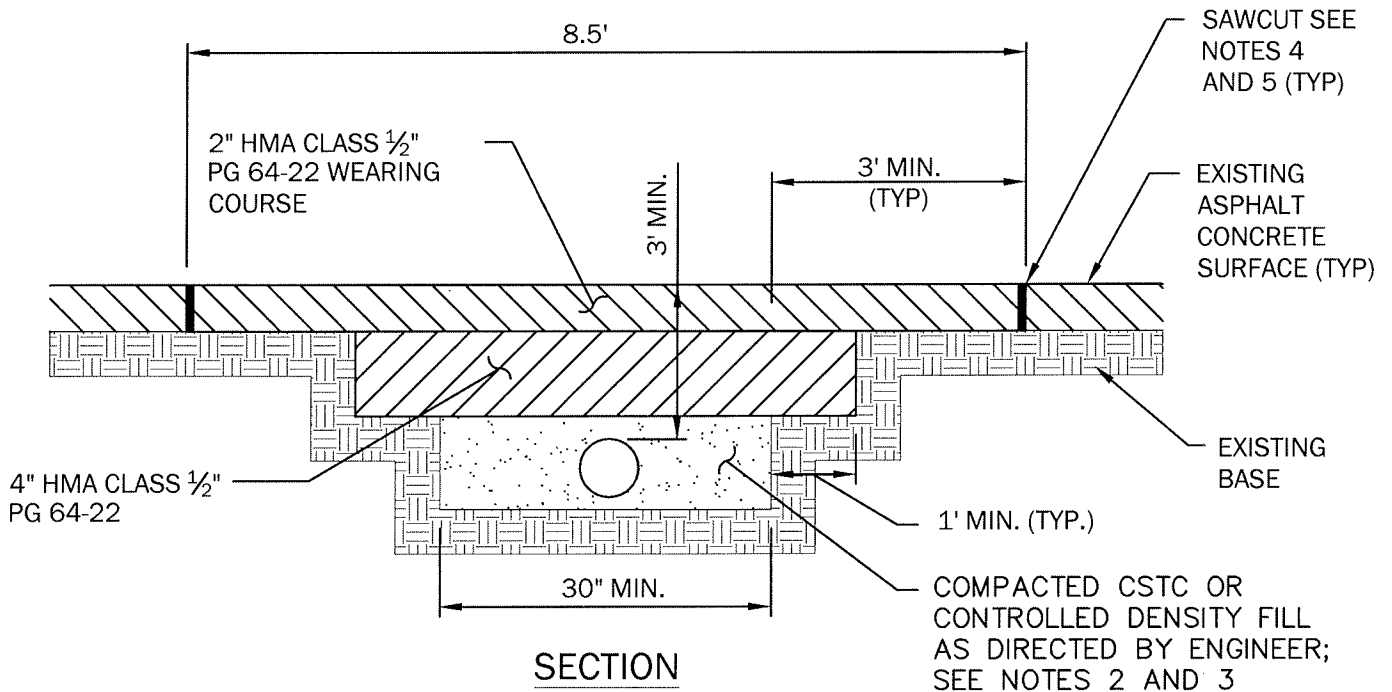


**ASPHALT PAVEMENT
 SECTION OF
 LONGITUDINAL CUT**

RD-13b

NOTES:

1. ASPHALT CONCRETE MIX SHALL BE HMA CLASS 1/2" OR CLASS 1" PG 64-22.
2. ALL TRENCH BACKFILL SHALL BE CSTC OR CONTROLLED DENSITY FILL.
3. CONTROLLED DENSITY FILL SHALL MEET WSDOT STANDARDS AS STATED IN 2-09.3(1)E OF THE STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION MANUAL M41-10, CURRENT EDITION.
4. ALL SAW CUTS SHALL BE VERTICAL AND IN STRAIGHT LINES AS DIRECTED BY ENGINEER.
5. TACK ASPHALT FACES OF SAW CUTS AND SEAL SAW CUTS WITH PG 64-22 OIL.
6. PAVING FABRIC (IF FOUND) WILL NOT REQUIRE REPLACEMENT.
7. HMA SHALL BE A MINIMUM OF 6 INCHES THICK ON LOCAL STREETS.



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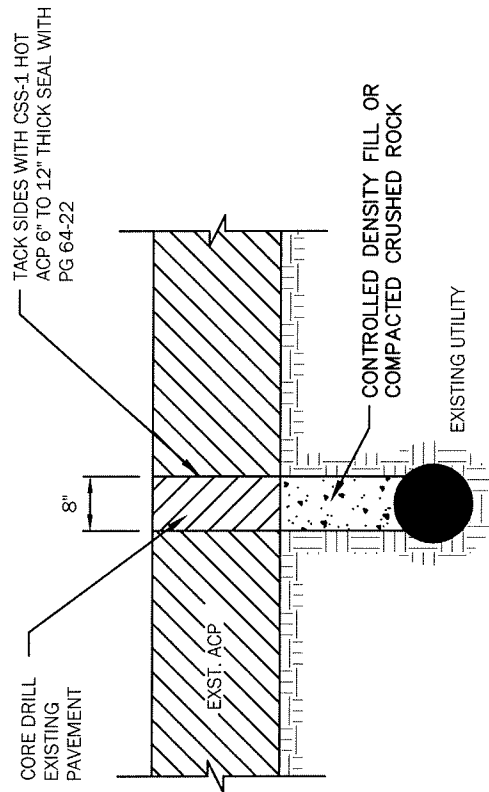
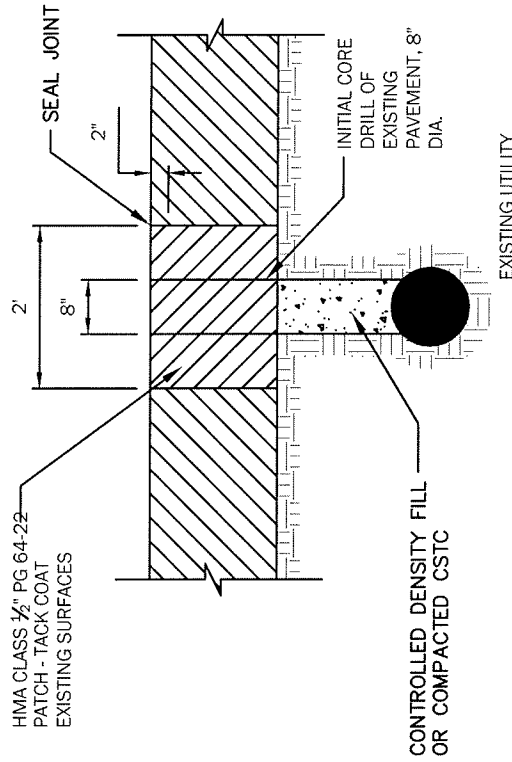


**ASPHALT PAVEMENT
PATCHING & RESTORATION
DETAILS
LONGITUDINAL CUT**

RD-13c

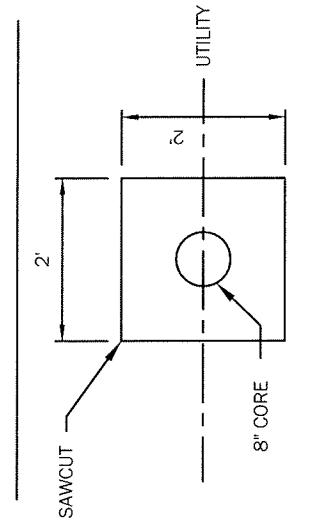
LAST REVISED: 04/06/17

ALTERNATE REPAIR
WHEN REQUIRED BY ENGINEER



NOTES:

1. THE EXISTING PAVEMENT SHALL BE CUT FULL DEPTH WITH AN EIGHT INCH DIAMETER CORE DRILL. THE SUBBASE MATERIAL SHALL BE REMOVED USING A VACUUM EXCAVATOR, KEEPING THE EXCAVATION AS MINIMAL AS POSSIBLE.
2. BACKFILL THE EXCAVATION WITH A SIX INCH CUSHION OF CRUSHED ROCK OVER THE UTILITY THEN PLACE THE REMAINING VOID WITH ODF OR COMPACTED CSTC.
3. REPAIR THE CORED PAVEMENT SECTION WITH HMA CLASS 1/2" PG 64-22 AND SEAL THE JOINT.
4. IF THE EXCAVATION BELOW THE ASPHALT PAVEMENT IS LARGER THAN THE 8 INCH CORE, THE PAVEMENT RESTORATION WILL INCLUDE A 2' BY 2' TEE PATCH FULL DEPTH OF THE ASPHALT CENTERED ON THE EXCAVATION, AS SHOWN ABOVE AS ALTERNATE REPAIR.
5. IF THE EXCAVATION IS LARGER THAN 2' BY 2', THE STANDARD GRIND AND OVERLAY RESTORATION SHALL BE USED.



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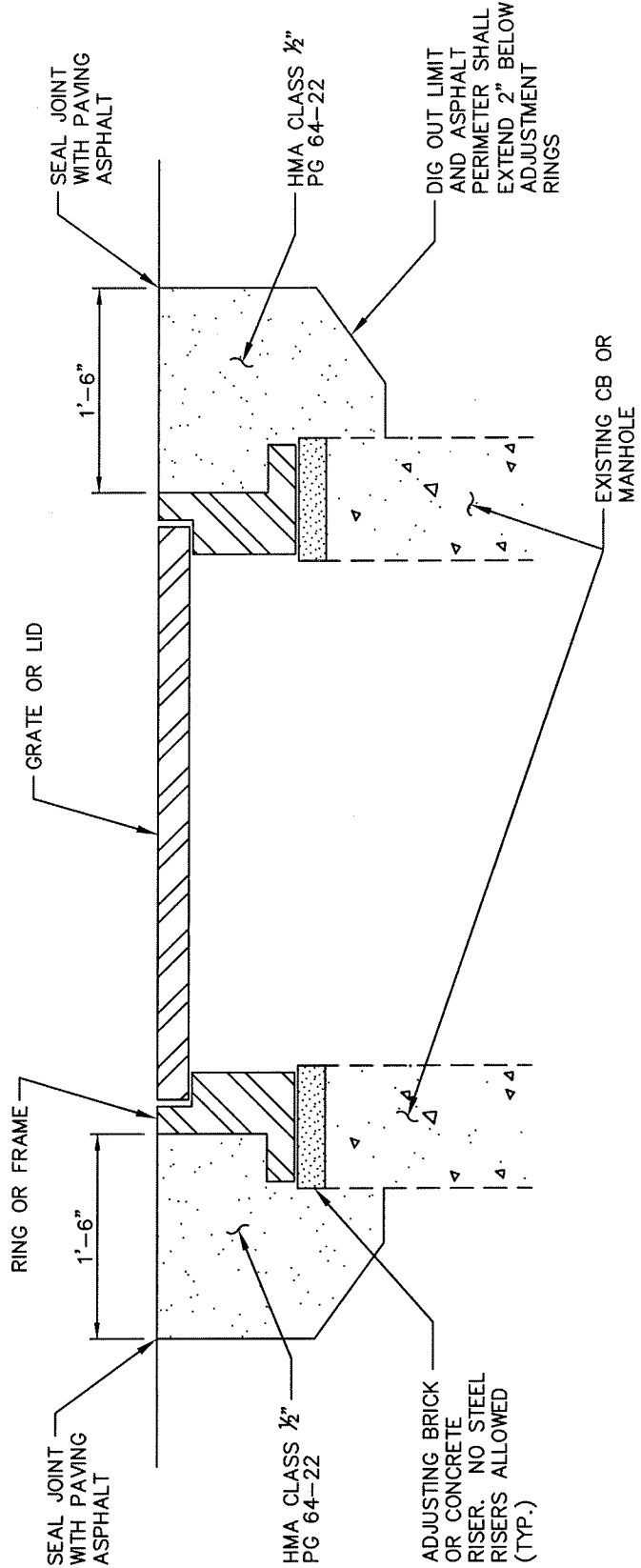


**ASPHALT RESTORATION
FOR WINDOW CUTS**

RD-13d

UTILITY MANHOLE AND VAULT ADJUSTMENT

THE EXISTING IRON FRAME AND COVER OR GRATE SHALL BE REMOVED AND THOROUGHLY CLEANED FOR REINSTALLATION TO THE NEW ELEVATION. THE EXISTING STRUCTURE SHALL BE RAISED OR LOWERED TO THE REQUIRED ELEVATION USING CONCRETE BLOCKS, BRICK, AND/OR CONCRETE RINGS. EACH JOINT SHALL BE GROUTED USING A ¾ INCH LAYER OF NON-SHRINK MORTAR, PLASTERED SMOOTH INSIDE AND OUT. COVERS SHALL BE SEATED ON A UNIFORM LAYER OF GROUT TO PREVENT ROCKING.



UTILITY ADJUSTMENT DETAIL

NTS

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ASPHALT PAVEMENT UTILITY ADJUSTMENT DETAIL

RD-13e

LAST REVISED: 04/05/17